



Operator Assault

Prepared by ETS Customer Safety & Security

A Summary of Operator Assaults for 2016

January 1, 2016 to December 31, 2016

Matthew Fitzgerald
Edmonton Transit Peace Officer Sergeant/Investigator
February 2017

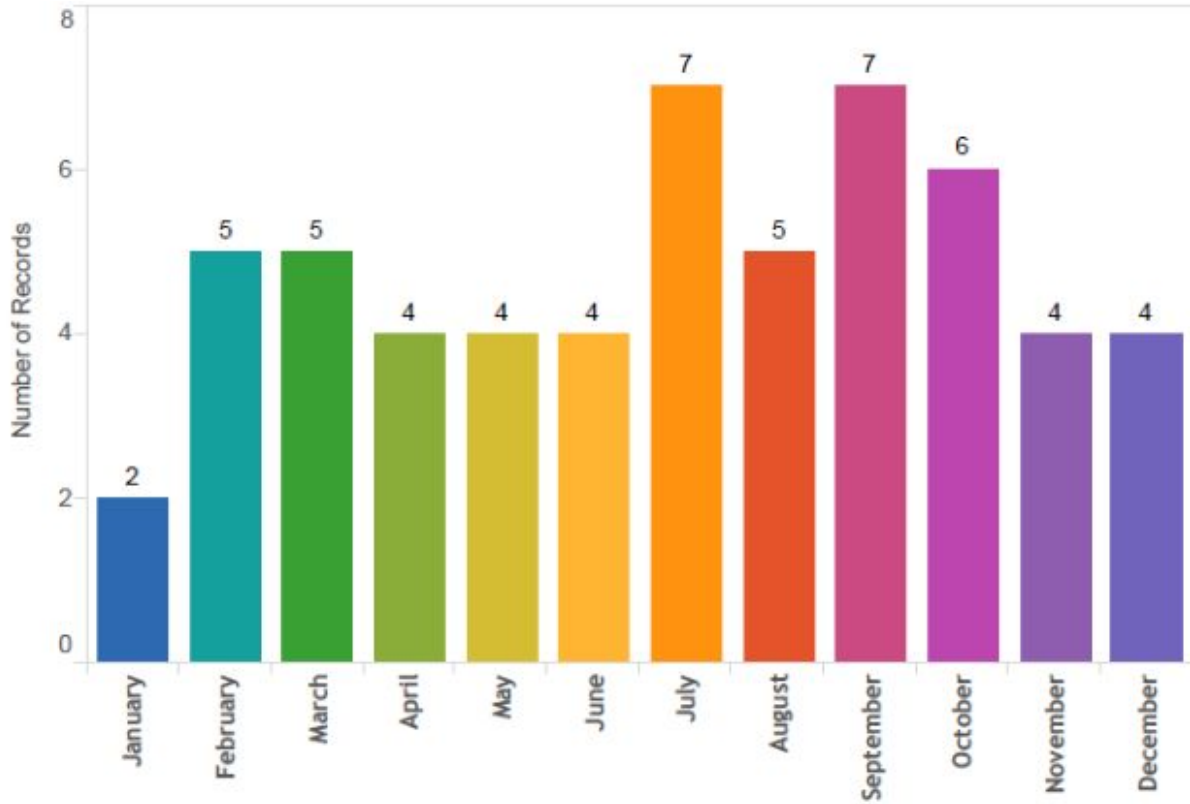
Approved: ETMT 2017 February 16

Summary

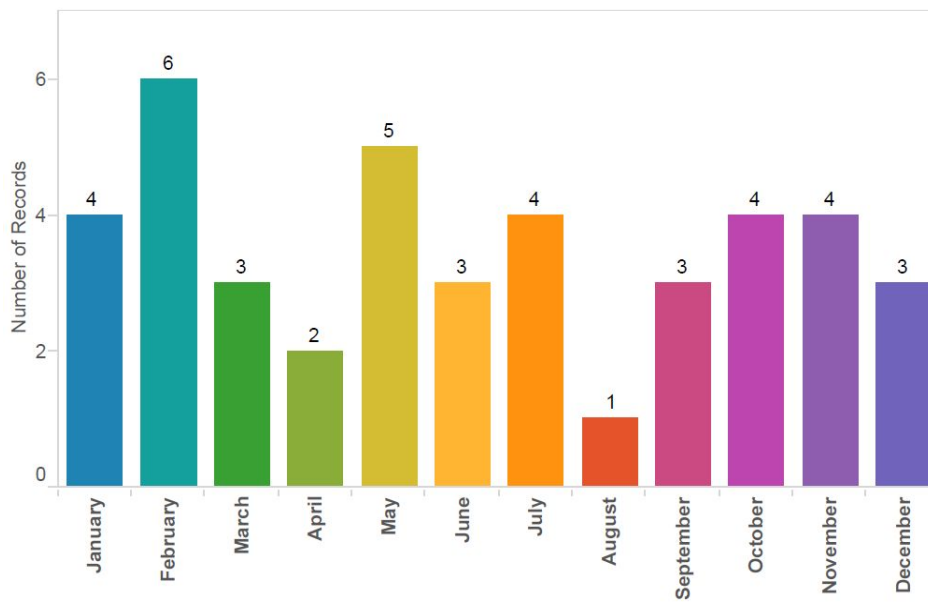
- Assault is defined within the Criminal Code of Canada and is categorized into three levels:
 - Level 1: Common assault (intentionally applying force by punch, kick, spit, push, grab, etc.)
 - Level 2: Assault with a weapon or assault causing bodily harm
 - Level 3: Aggravated assault with the intent to harm, maim or endanger the life of the victim
- 57 assaults on Bus Operators were recorded by ETS Customer Safety and Security in 2016. For an incident to be recorded and classified as assault, the circumstances must align with one of the aforementioned levels and corresponding offence section as defined within the Criminal Code of Canada.
- Reported incidents that did not meet these criteria and criminal charges deemed not probable following Edmonton Police Service (EPS) investigations were recorded as something else such as conduct of person - harassment, and bylaw action taken and/or bans issued.
- To compare 2015-2016 statistics: In 2016 there were 57 total recorded assaults, 33 (58%) suspects identified. In 2015 there were 42 total recorded assaults, 28 (67%) suspects identified.
- 55/57 Assaults in 2016 were Level 1 and 2/57 Assaults were Level 2 (Assault with a weapon or causing bodily harm), one of which a female suspect struck the operator on the knee with a metal pipe and the other, a male suspect lunged at the operator with a chisel. No Level 3 Assaults reported.
- 30% of total recorded assaults in 2016 were the result of a fare dispute in some form which varied from reasons such as not allowing a customer to ride for free, not providing a transfer, customers using expired fare or not producing their fare. This number is down from 52% in 2015 but is still the main probable cause of assaults.

- 26% of total recorded assaults in 2016 were the result of an operator confronting a disruptive customer over their behaviour. This number is up from 19% in 2015 and remains the second highest probable cause of assaults.
- Determining the preventability of an assault is speculative as it assumes that an operator's inaction or submission to the assailant will prevent the assault from occurring. Based on these assumptions, approximately 55% of the assaults in 2016 were preventable.
- 58% of suspects were identified, either shortly after the incident or at a later time through CCTV. 39 out of the 57 buses were video equipped (68%). 16 suspects were identified after the fact from video obtained from the bus and there were 7 other incidents where the bus was video equipped however the suspect was arrested at the time and 9 other incidents where the bus was not video equipped however a suspect was arrested at the time. 1 Suspect was identified after the fact through LRT Station video.
- Some assaults in the "unprovoked" category may have occurred for a specific reason however POSSE files do not contain information to indicate this. This may be due to information not being obtained by investigating TPO's or the operator not stating or being entirely sure of a reason.
- Contacts with Bus Operators (CBOs) and Rides on Buses (ROBs) show the number of CBOs and ROBs that occurred on the top 10 ETS bus routes and the total amounts completed for 2016 (38,519 CBOs & 8,489 ROBs).
- The three routes with the highest number of operator assaults were the Route 8 (9 assaults), Route 1 (7 assaults) and Route 9 (7 assaults). There were a total of 3,316 CBOs & 597 ROBs on the Route 8; 2,234 CBOs & 1,392 ROBs on the Route 1; 2,499 CBOs & 290 ROBs on the Route 9 for 2016.

2016 Assaults by Month



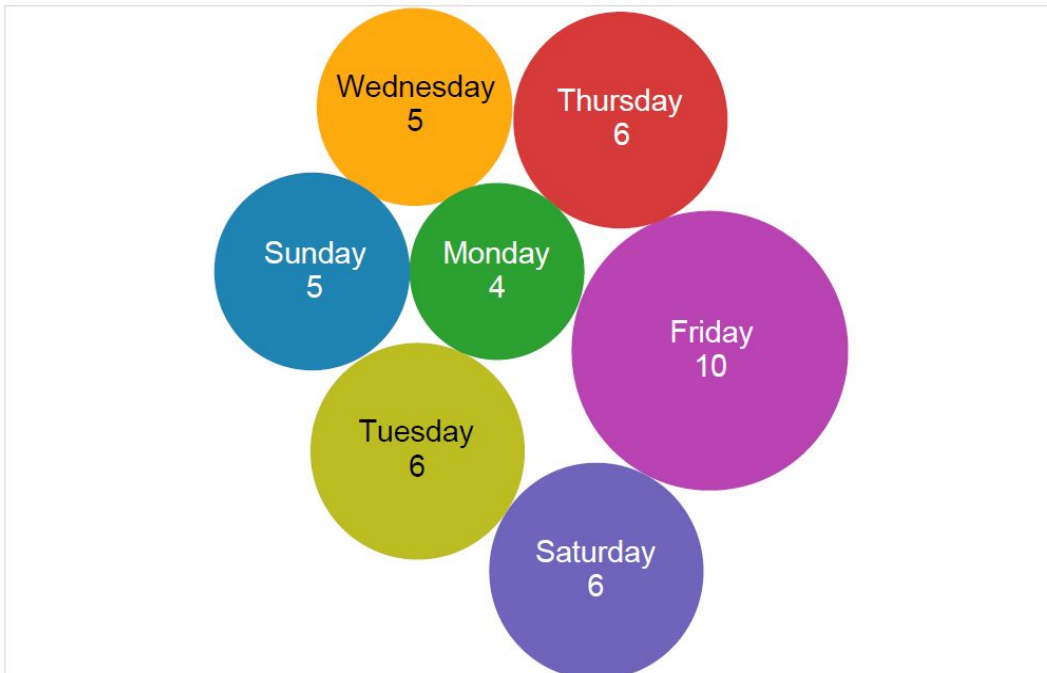
2015 Assaults by Month



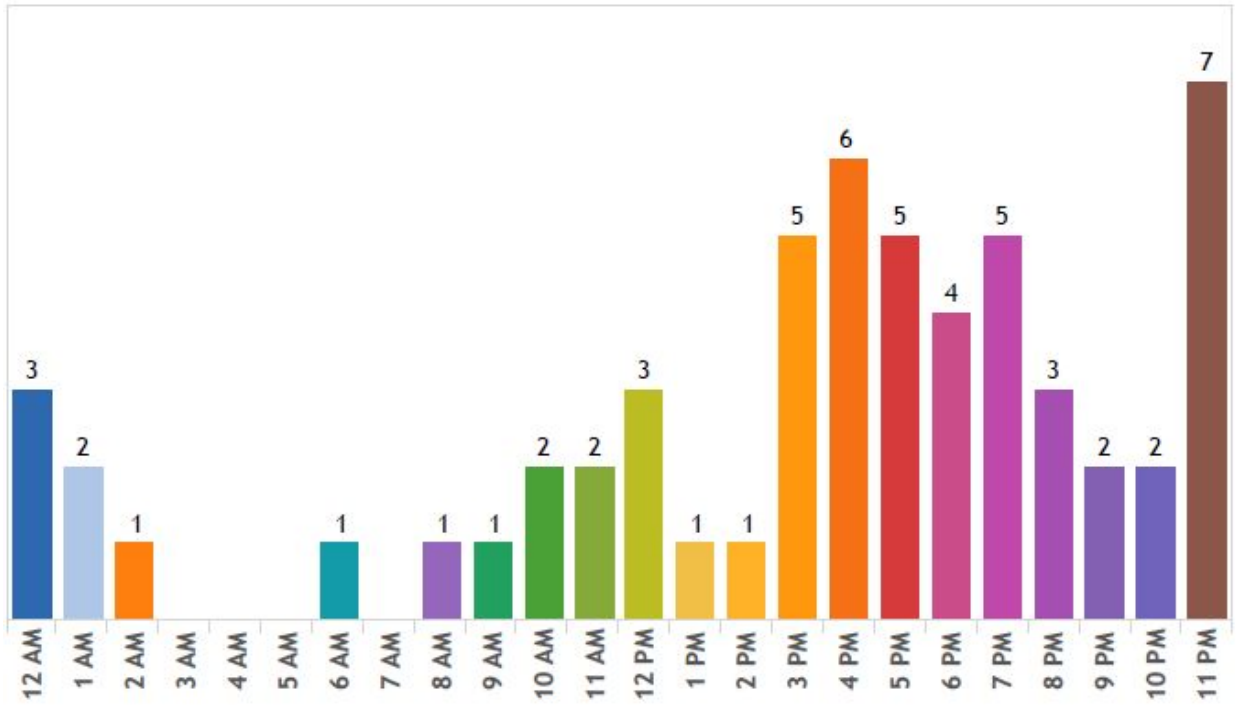
2016 Assaults by Week Day



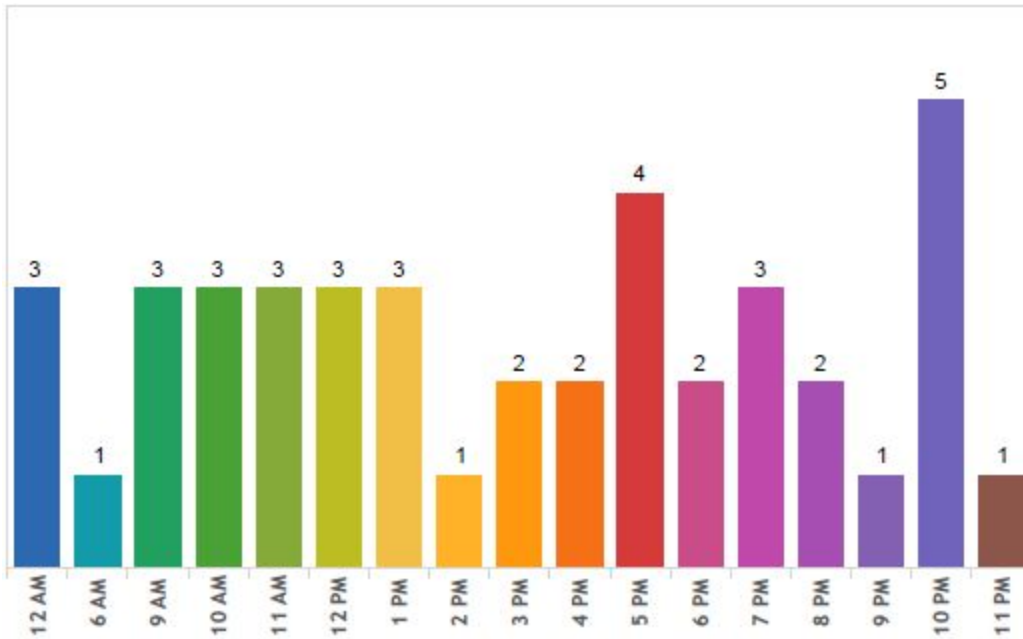
2015 Assaults by Week Day



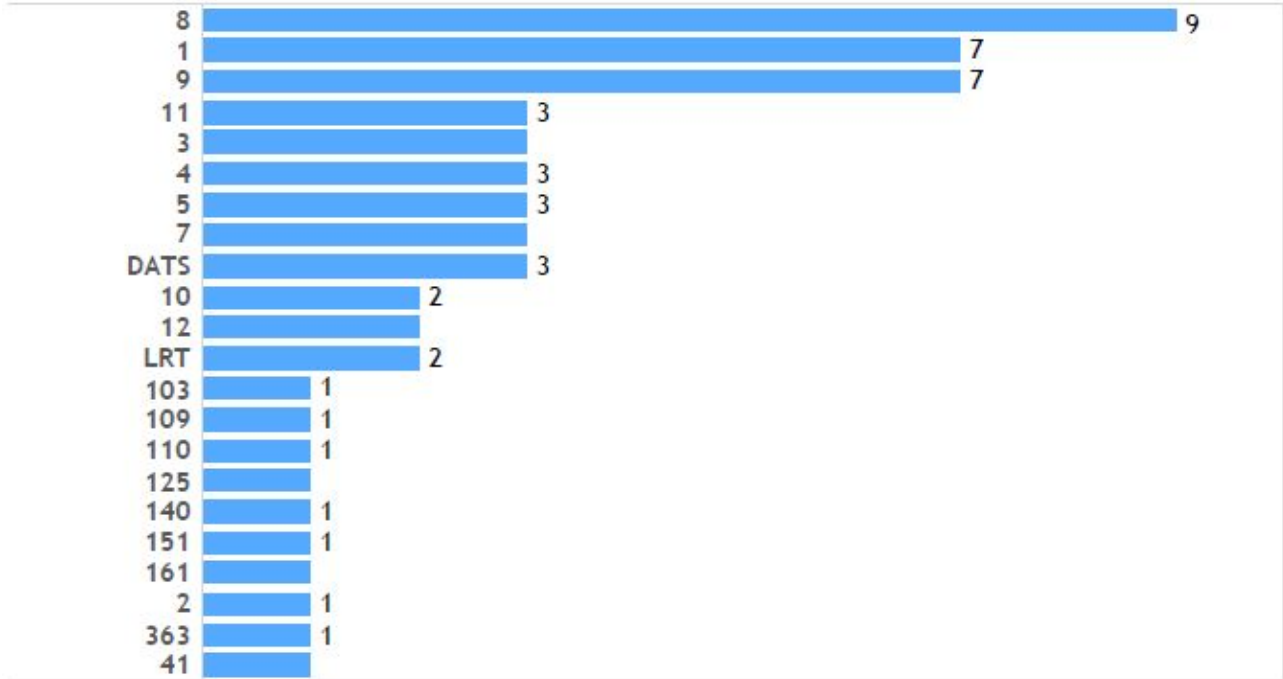
2016 Assaults by Hour



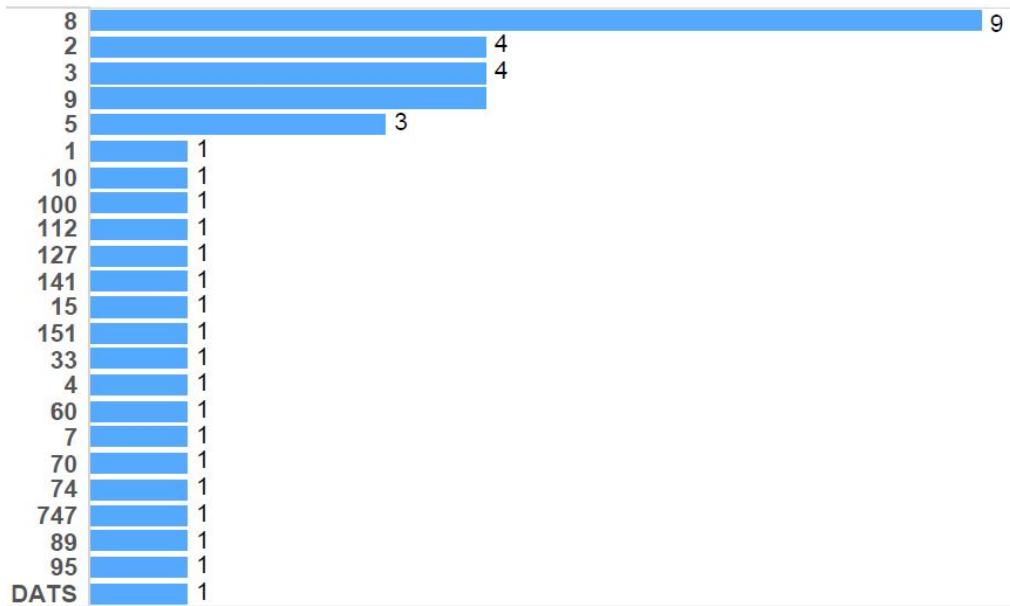
2015 Assaults by Hour



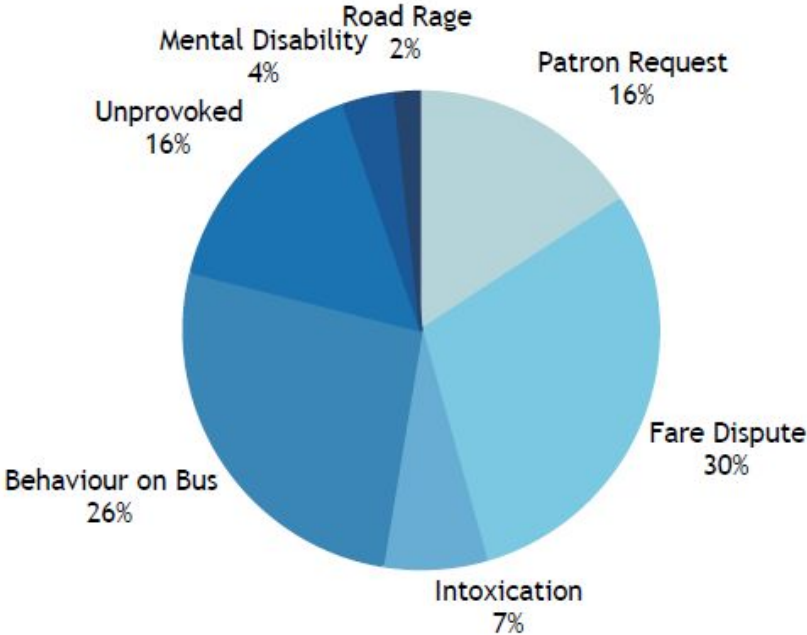
2016 Assaults by Route



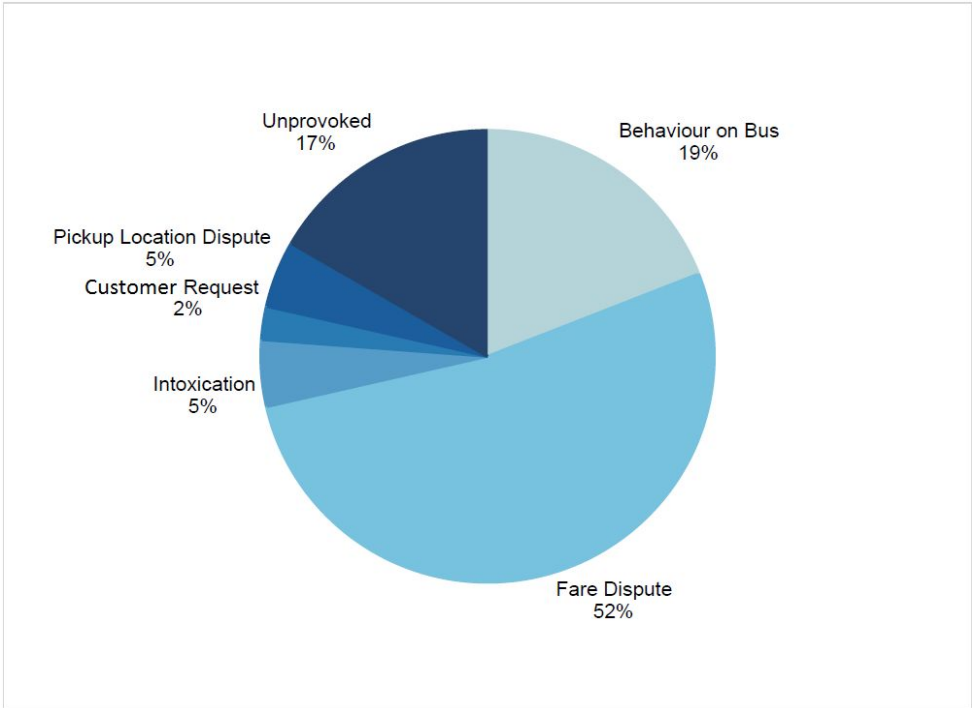
2015 Assaults by Route



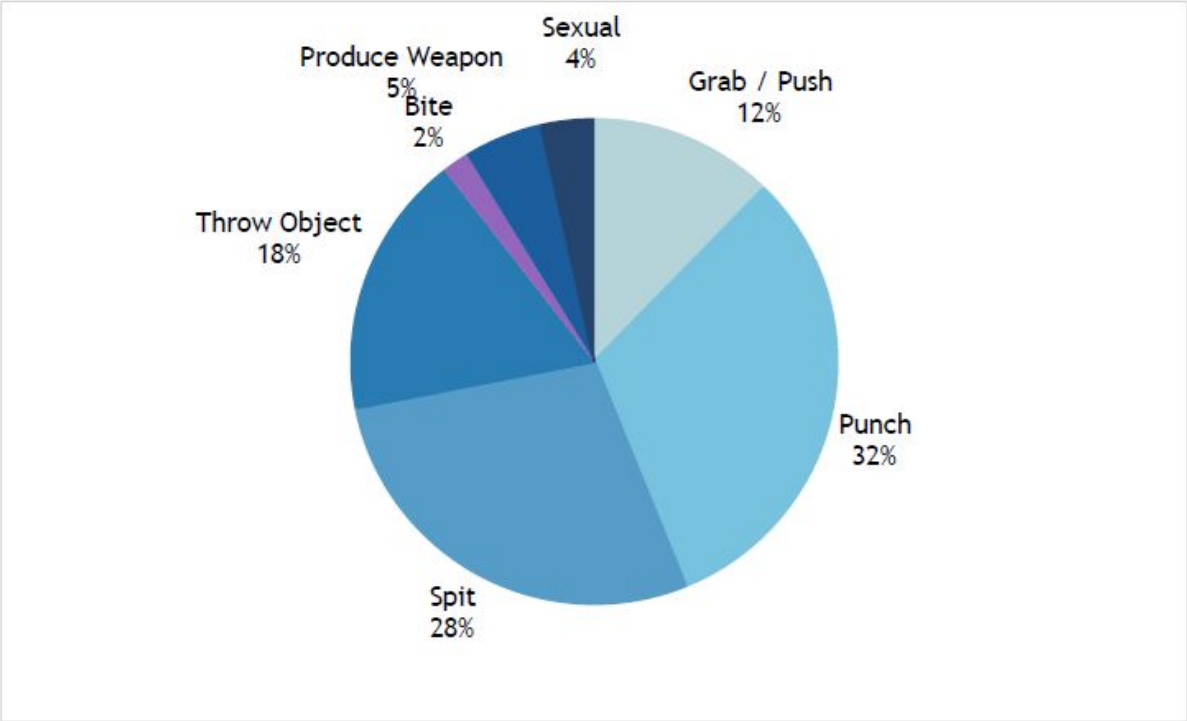
2016 Probable Cause



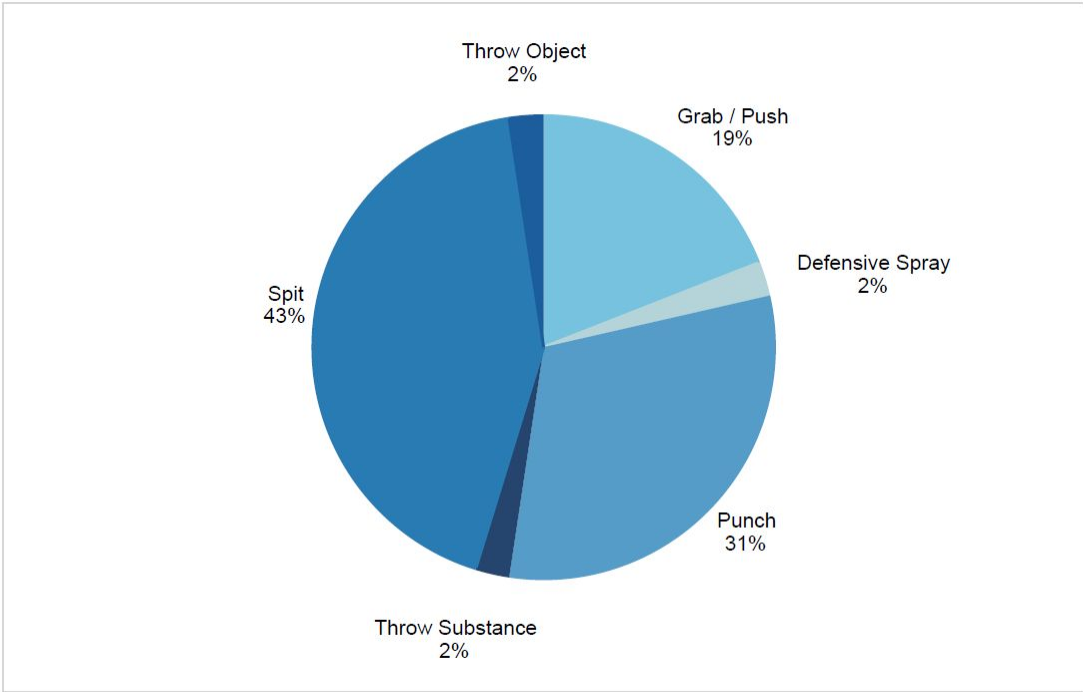
2015 Probable Cause



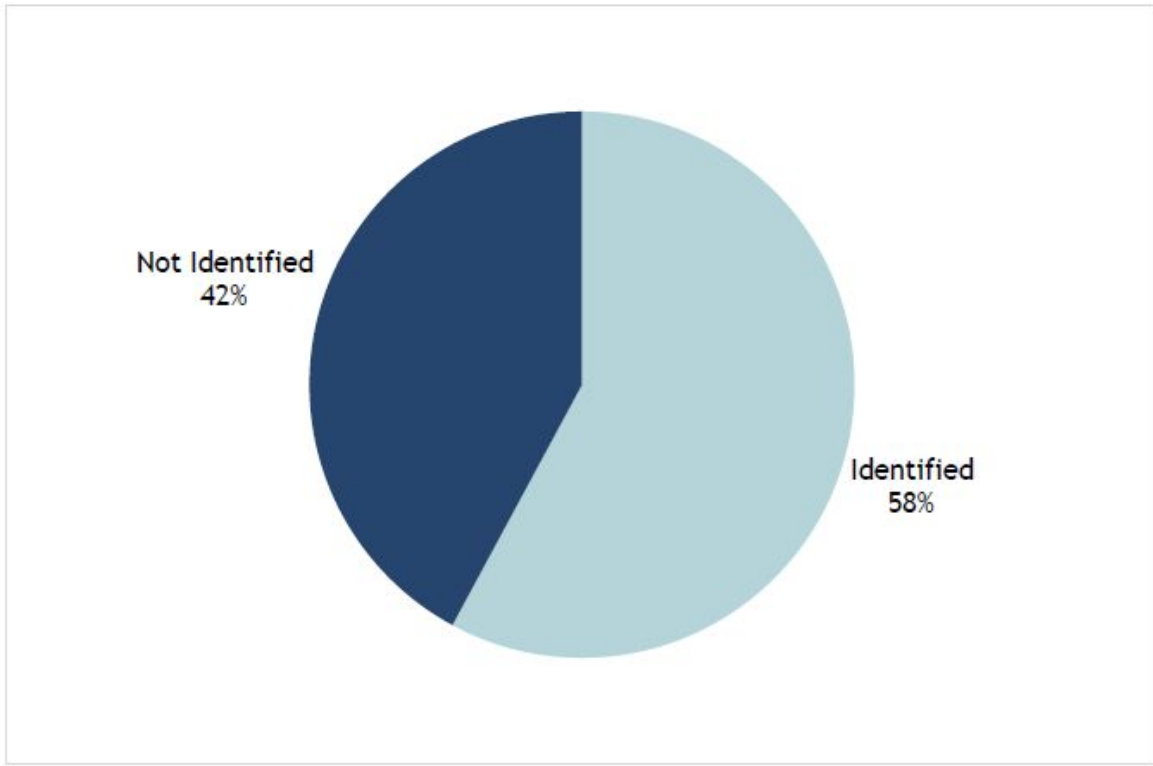
2016 Nature of Assault



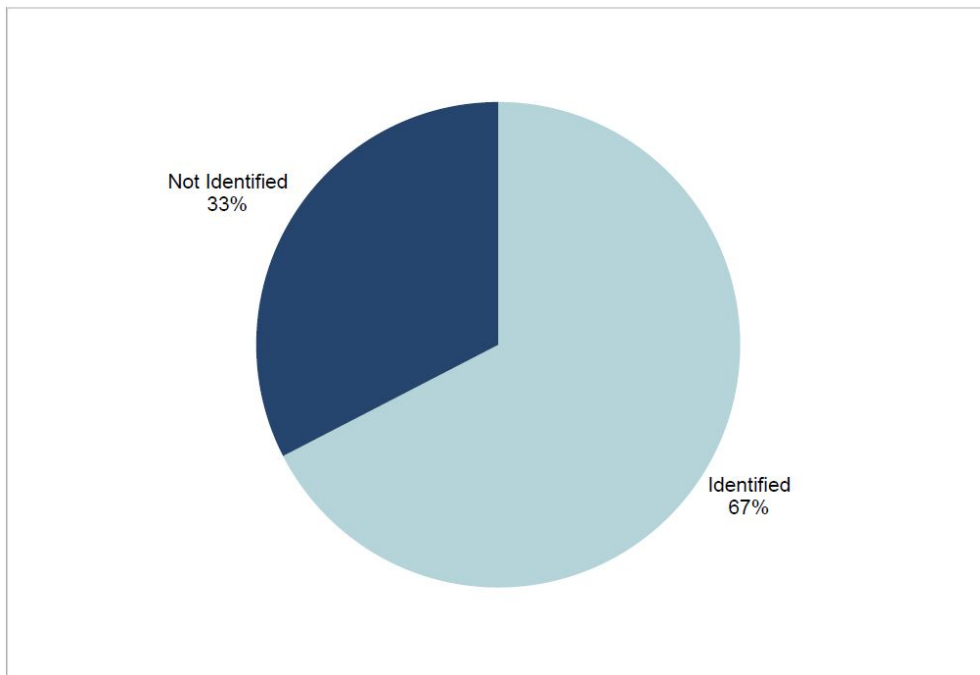
2015 Nature of Assault



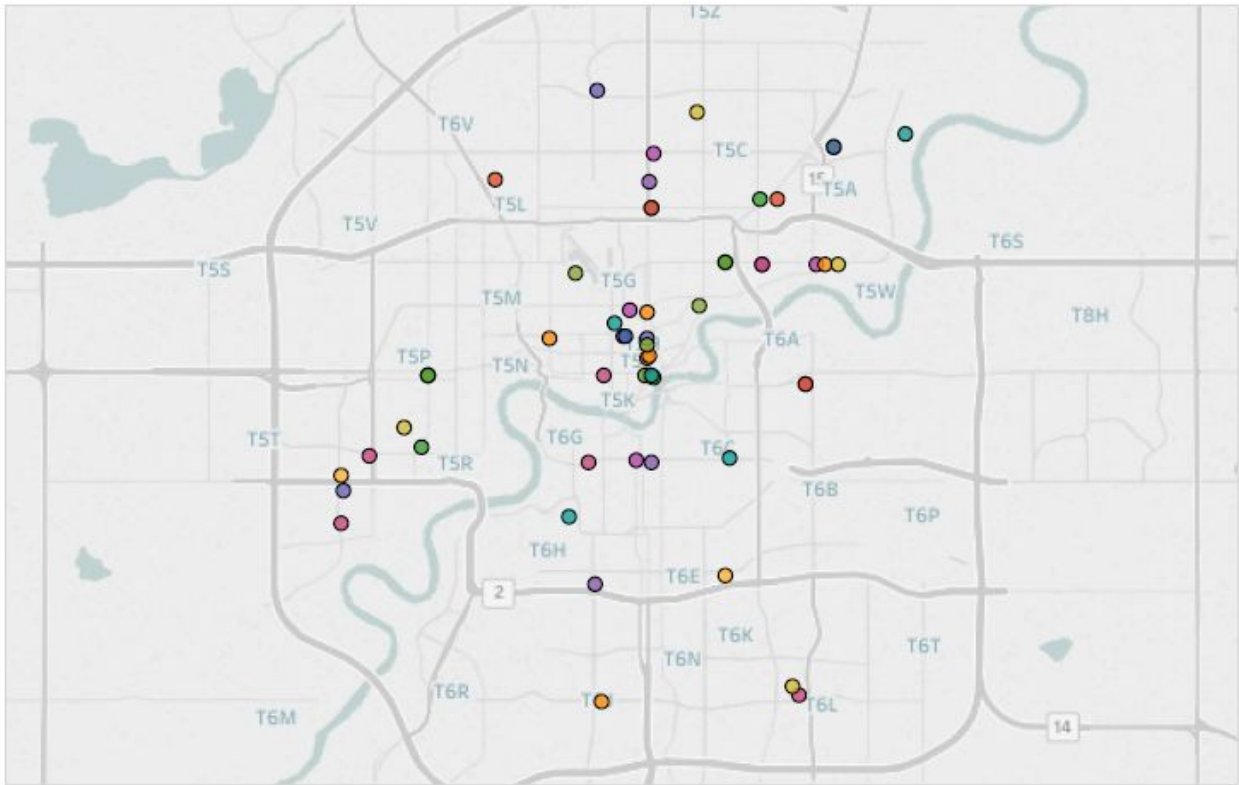
2016 Suspect Identification



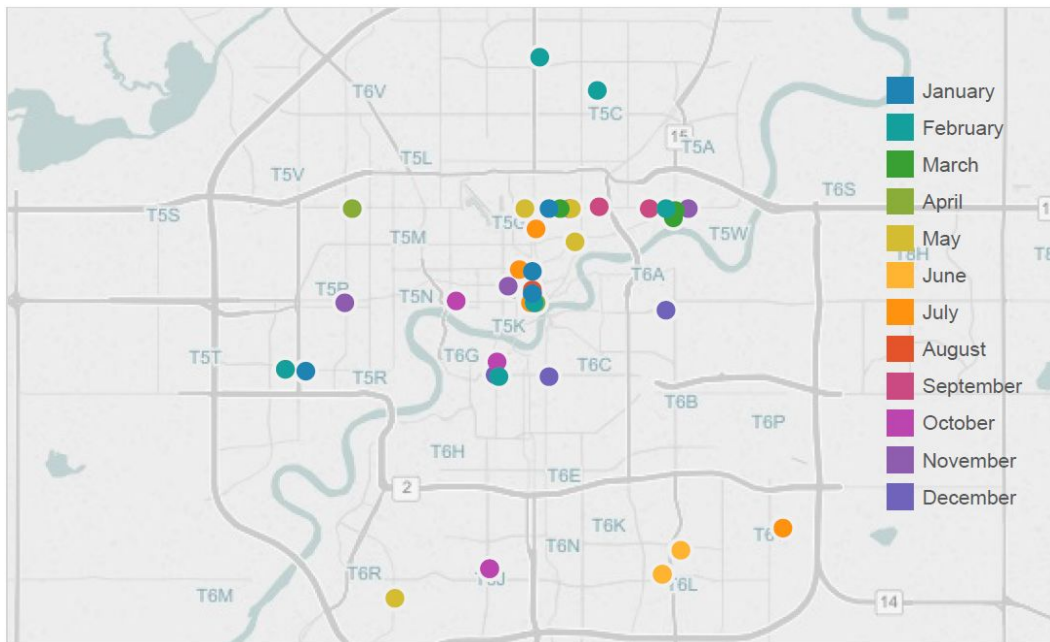
2015 Suspect Identification



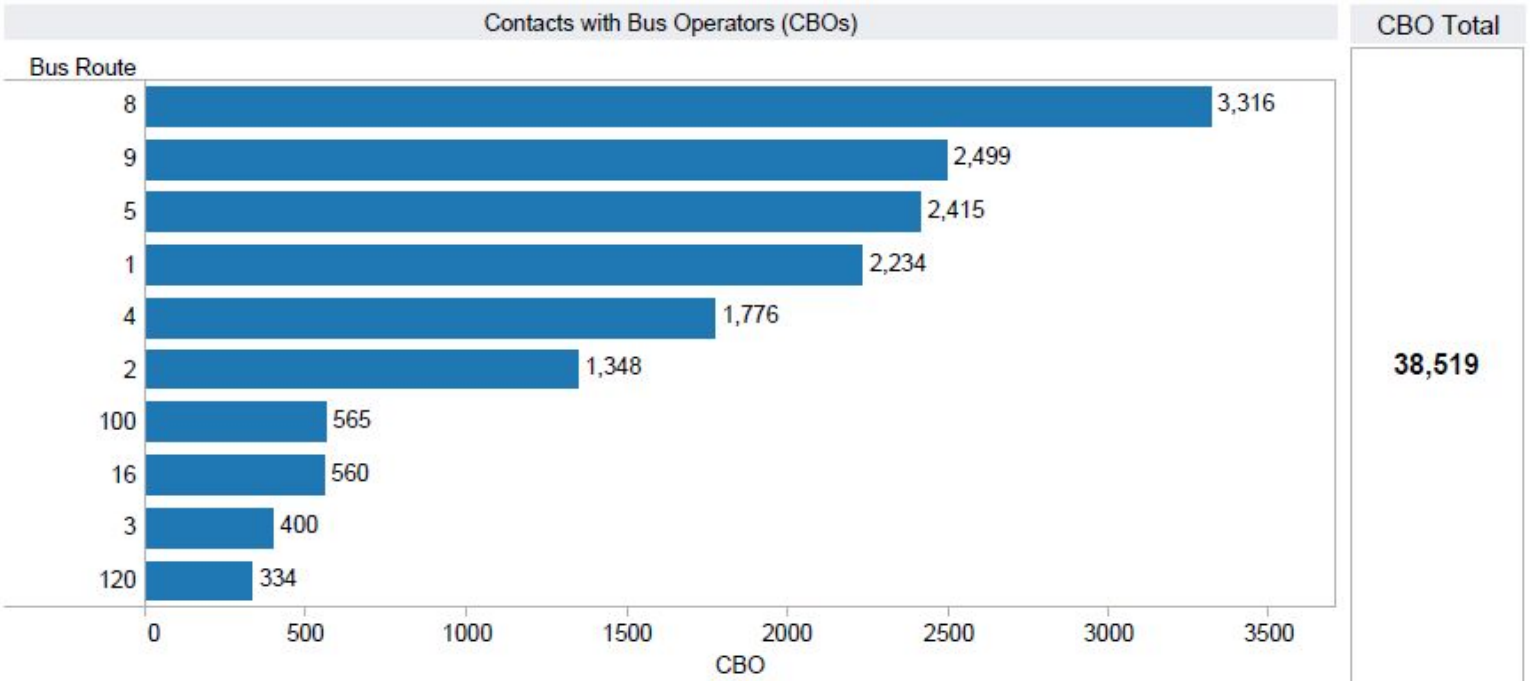
2016 Incidents Map



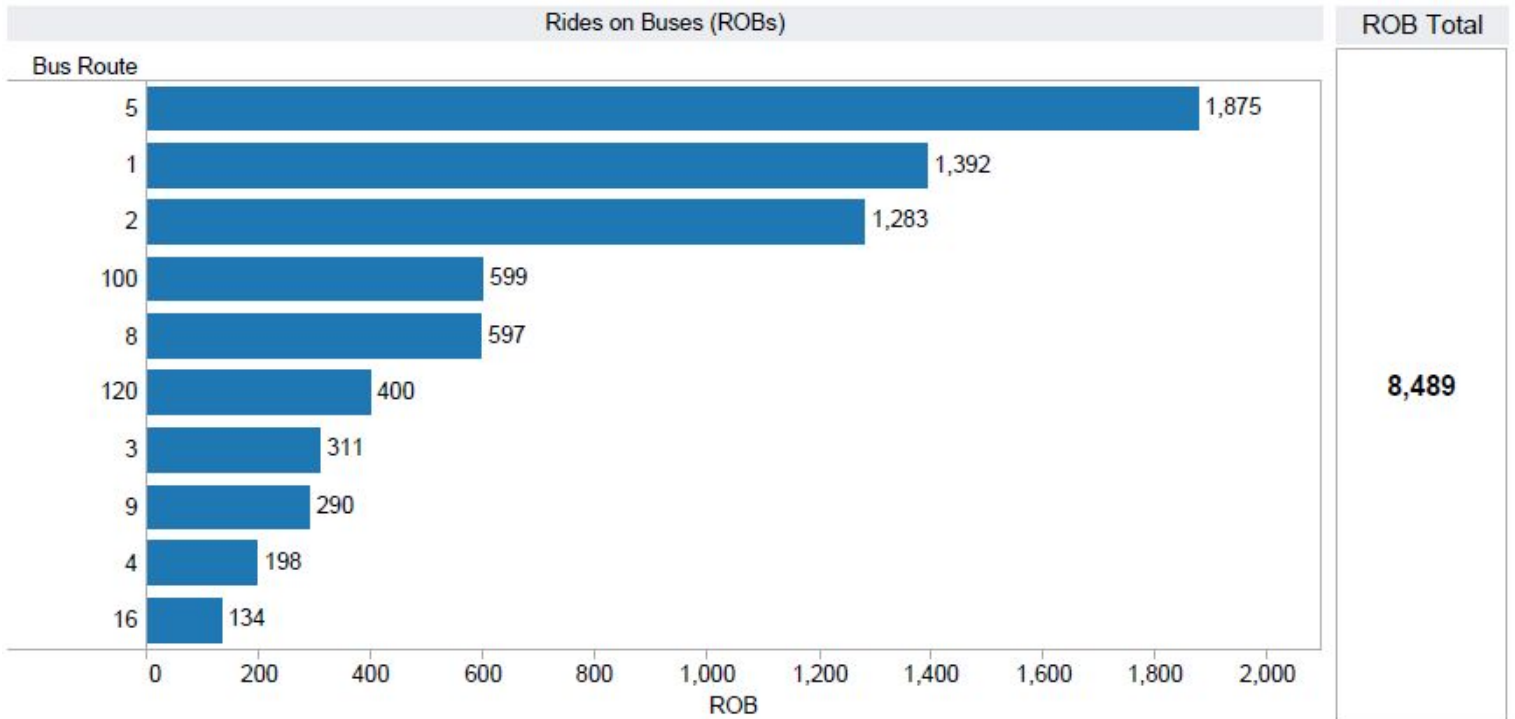
2015 Incidents Map



2016 Contacts with Bus Operators (CBOs)



2016 Rides on Buses (ROBs)



2016 Disposition of Incidents

Disposition of Incidents	Count	% Total
Criminal Code charges (Assault)	24	42.1%
Suspect not identified	24	42.1%
Bylaw summons	3	5.3%
Suspect identified - Operator declined enforcement action	3	5.3%
Disabled Adult Transit Service (DATS)	3	5.3%
TOTAL	57	100%

- 24 Incidents involving Edmonton Police Service (EPS) saw Criminal Code assault charges laid either on the date of the incident or at a later time, after a suspect was identified from video obtained from the bus and suspect information passed on to EPS. 24 incidents saw no enforcement action as the suspect was not identified.
- 3 Incidents did not meet the criteria for a criminal code assault charge however the suspect was issued a bylaw summons instead.
- 3 Incidents saw no charges laid due to the operator not willing to attend court either for a criminal charge or bylaw summons.
- 3 Incidents involved Disabled Adult Transit Service (DATS) operators and in all of these incidents, no criminal charges were laid or bylaw summons issued due to suspects suffering from cognitive and/or mental disabilities.